

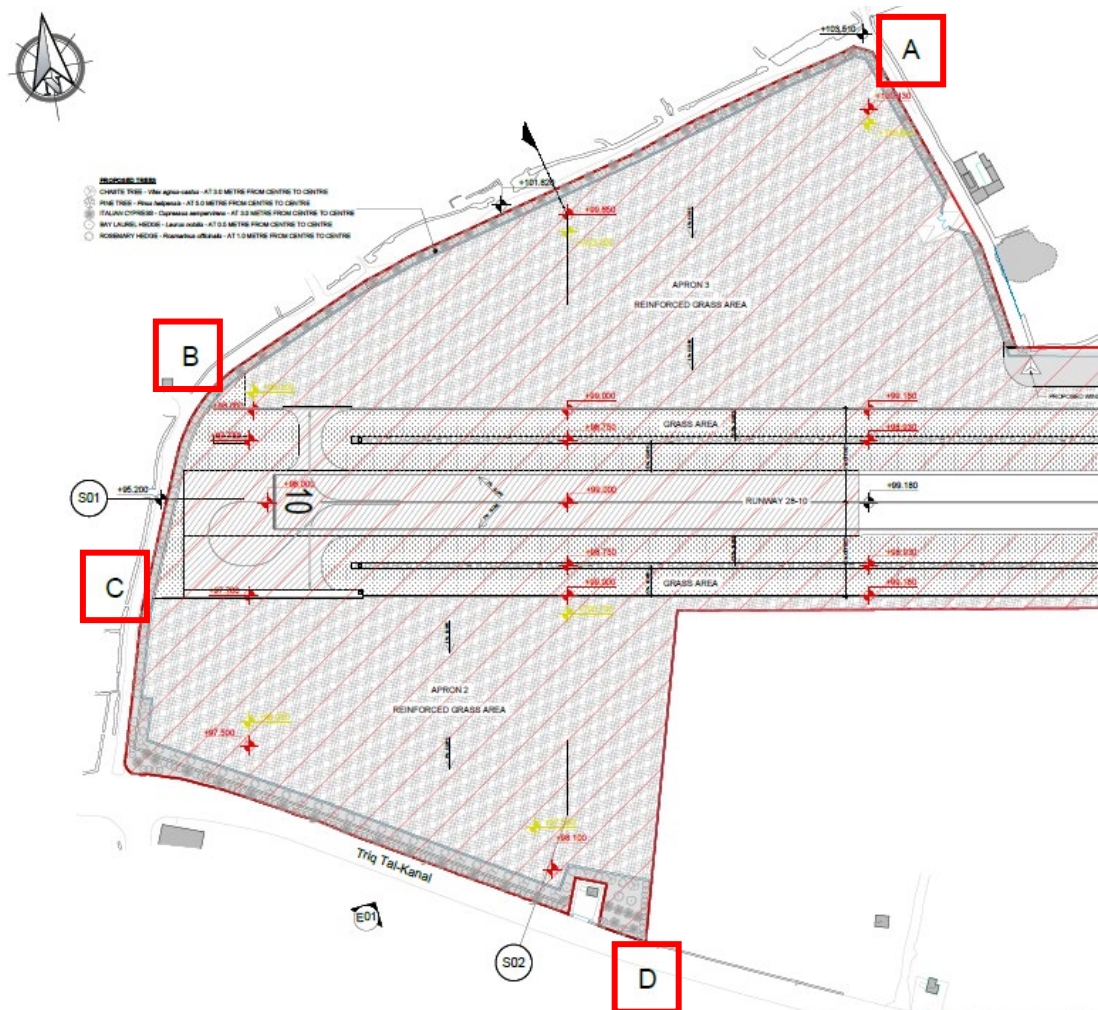
08 July 2024

Application Number: PA/07333/22

Location: Gozo Rural Airfield, Triq ta' Lambert, Ghajnsielem

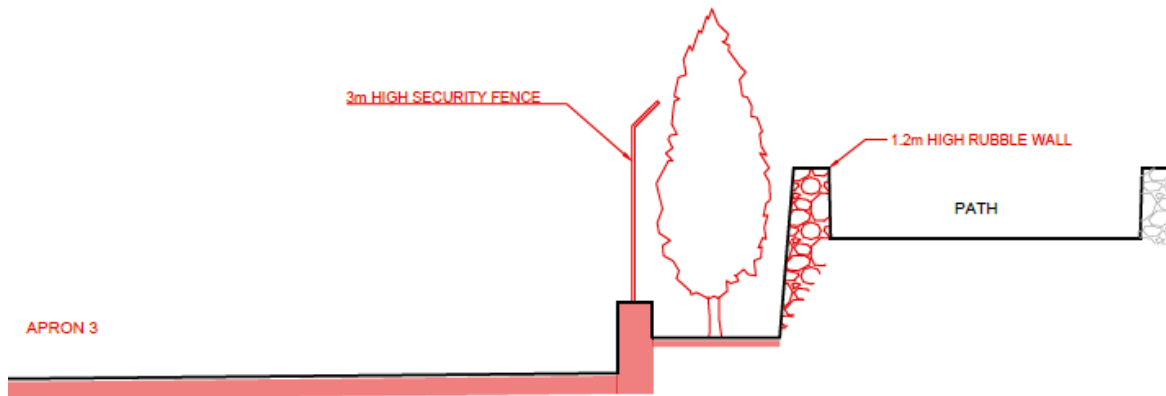
Proposal: Proposed upgrading of existing airfield including the extension of the runway, construction of a concrete-surfaced apron, three grass-reinforced aprons, perimetral retaining walls, shifting of security fence, new landscaping, addition of vehicular parking spaces and supporting facilities at basement level, and sanctioning of illegalities mentioned in ECF 579/10.

With particular reference to the area along Threshold 10 (i.e. along the west of the site in caption), the undersigned Perit confirms that the north, west and south boundary of the site has been detailed in such a way as to create a physical barrier by the provision of (a) perimeter walls / security fences and (b) landscaping, as per drawings submitted to the Planning Authority, and explained below:



### North Boundary (Between Points A to B, refer to Drawing 1 above)

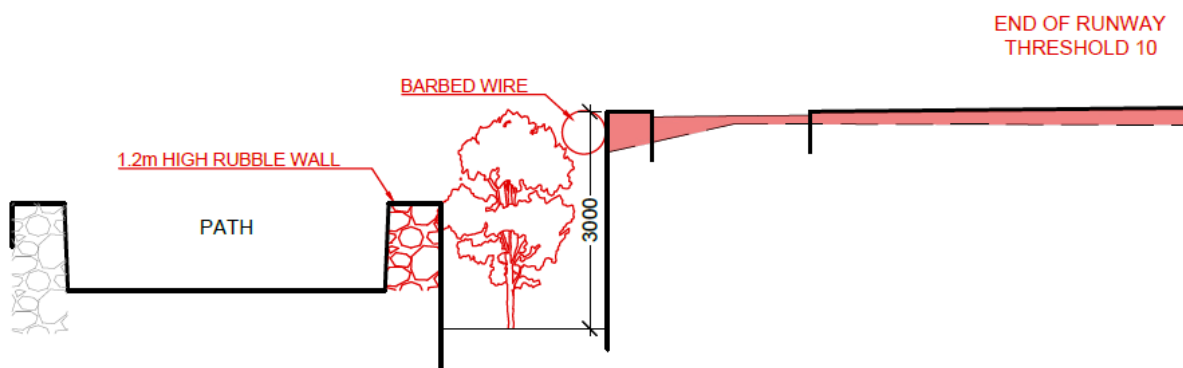
The design of the north boundary of the site takes into account the change in elevation (height) between the existing levels of the pathway and the proposed levels of Apron 3. This boundary wall detail, as shown in Drawing 2 below, incorporates a retaining wall with a 1.20m rubble wall along the path, and an inner dwarf wall (1.0m high) with an overlying 3-m high security fence. A landscaping buffer zone is being proposed as shown.



Drawing 2: Section along the North Boundary of the site.

### West Boundary (Between Points B to C, refer to Drawing 1 above)

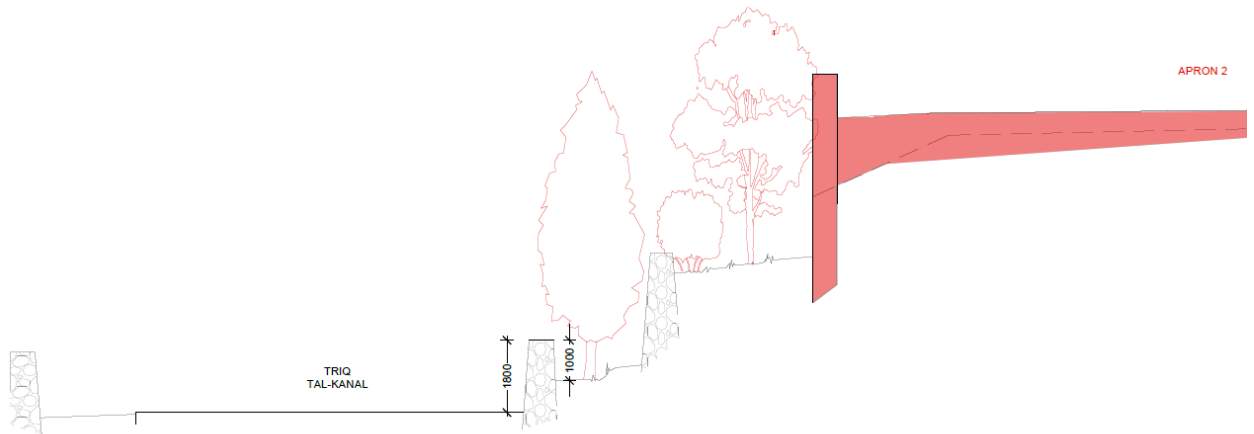
The design of the west boundary interface of the site takes into account the exclusion of any protruding obstructions to be proposed in front of the landing/taking-off zone of the extended runway, i.e. along Threshold 10, as shown in Drawing 3 below. To this effect, site security has been achieved by proposed a lowered landscaped buffer zone, incorporating a retaining wall along the runway edge, and a landscaped buffer with a 1.20m rubble wall along the existing path.



Drawing 3: Section along the West site boundary.

**South Boundary (between Points C to D, refer to Drawing 1 above)**

The design of the south boundary of the site takes into account the change in elevation (height) between the existing levels of the public road (Triq tal-Kanal) and the proposed levels of Apron 2. This change in level is bound by the maximum permissible gradients of the apron (in accordance with ICAO Standards) and which cannot be lowered any further than that shown. As shown in Drawing 4 below, a stepped landscaped buffer zone is being proposed, which incorporates a retaining wall with a 1.0m high overlying dwarf wall, and a stepped landscaping buffer along the public road.



Drawing 4: Section along the South Boundary of the site (along Triq tal-Kanal).

Perit Ramon Fenech  
**QPM Limited**